



**GRADY-WHITE BOATS**

[gradywhite.com](http://gradywhite.com)





Eddie Smith, CEO. Kris Carroll, president

# *Passion* for our customers and team: Building in quality, reliability and safety

Thank you so much for considering the Grady-White boating experience for your family. While detailed current specifications about our products are best found at your Grady-White dealership or at [www.gradywhite.com](http://www.gradywhite.com), we invite you to enjoy this guide that gives an overview of our products, a sense of who we really are, and how we delight in creating your ultimate boating experience.

Eddie Smith has owned Grady-White Boats since 1968. Kris Carroll

joined Grady-White in 1975 and has been nurturing customer care and teamwork as president of our company since 1993, through a combination of “building” our people as well as our products, and investing in their lifelong learning. Grady-Whites are created by the very best craftsmen in the most complete and modern fiberglass boat building facility

A relentless  
passion to do the  
“right thing” by and  
for our customers

of its kind in the world. We believe that our investment in our team in addition to our facilities is the underlying principle that helps us meet our goal of providing the ultimate boating experience. We call this “Grady Built.”

A combination of things makes our company different. We have a relentless passion to do the “right thing” by and for our customers, a dedication to sportsmanship and fish-

eries conservation, a strong principle of “safety first,” and a keen understanding of “The Grady Life” lifestyle. In the end, today’s Grady-White boat ownership experience is a product of truly listening to customers. Our associates are out in the field gathering customer feedback and our customer relations folks have hundreds of phone and email communications with customers every week! Through these interactions, we learn more about our customers and how to meet and exceed their expectations.







Marine Advisory: Get the Grady! Built with quality and reliability for your safety and peace of mind.

We strive to have what are arguably among the closest relationships with customers in American business, AND we are set apart for having a passion for building a team that has the information, the tools and the motivation to take the quality of our products to levels that are simply unattainable for most companies, in any industry. That passion is the reason the majority of our workforce has been with us 10 years or more. Our internal team goal is that we “enjoy coming to work on Monday like we enjoy going home on Friday afternoon.”

That passion is why our quality measurements surpass even the most stringent goals. Pride of workmanship

A heritage of sportsmanship, fisheries conservation and environmental stewardship is a part of every Grady-White.

isn’t a thing of the past; it is a living, breathing part of every day at Grady-White. And even though our boats are incredibly feature rich and full of many complex systems, our team continually produces extraordinary quality.

We are delighted to provide you, our customers, with “the ultimate boating experience” and help you create life-

long memories. Please come and join the over 500 individuals, families and groups who tour the plant each year and see for yourself the astonishing associate satisfaction that yields such quality. A lot of companies talk about quality, but Grady-White associates truly deliver. Ask Grady owners—they are raving Grady-White fans!



# GRADY-WHITE

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# Superb engineering:

Design inspired by customer input

The people who design and engineer Grady-Whites not only are boaters themselves, they and the entire management and customer relations crews are constantly in contact with Grady-White customers out on the docks and

on the water, through Grady Clubs, get-togethers, fishing events, boat shows, owner forums, phone calls, emails, surveys, dealer events, dealer seminars, publications, websites, blogs, social media and—best of all—plant

tours. The flow of information and support is constant. Customer input inspires our design.

The Grady-White team is tireless in designing the features that customers put on their wish lists and really want to enhance their boating experience.

What's more, customer relations, engineering and production—every single associate—participates

in weekly updates

about customer,

dealer, and supplier

feedback and internal quality initiatives, resulting in a constant incorporation of owner-driven designs. What all this means is that every feature of every boat is where and what it is because of merit to real customers. Thus, features such as our specialty storage, built-in rod holders and toe rails, to name just a few, are truly functional and add to

fishing and boating versatility. Adjustable helm seats are a must-have specifically desired by customers, and are continuously upgraded to make sure we are hitting the mark.

Grady-White designers and engineers are not only experts at translating customer ideas into solid features; they are superb with the software used to

deliver the precision that customers expect from Grady-White.

Our mechanical

and structural engineers are exacting and experienced. Fiberglass molds are made from CNC machined plugs: the smooth finish on Grady-White hull sides and the well-engineered fit and finish throughout the boat result from this first step of superb woodcraft. And since 1988, the naval architecture firm of C. Raymond Hunt Associates has

Customer input  
inspires our design.

Multipurpose use features like a cockpit side door work well

for many water activities, and Grady-White always has fishing function in mind.





Customers want openness and protection, and Grady-White responds with the exclusive Airview<sup>2</sup>™ (AV<sup>2</sup>™) hardtop enclosure that achieves this beautifully and offers so much more.



worked with Grady-White to create the exclusive SeaV<sup>2</sup>® Grady-White running bottoms, as well as provide advice on structure and materials. All of today's Grady-Whites have SeaV<sup>2</sup> hulls.

We regularly call upon other world-class structural engineers, mechanical engineers, chemists, and ergonomics

experts. Grady-White proofing involves everything from stress testing laminates to corrosion testing batches of stainless steel. But the best testing lies right outside our doors in North Carolina's sounds, inlets, capes and offshore waters. We engineer our boats to perform superbly even in the toughest conditions. The sea does not compromise, so neither do we on integrity, quality, reliability and safety.



Customers tell us they want handy yet out-of-the-way storage. A sliding storage drawer (useful for fishing tackle) is one result.





# *Passionate precision:* Cultivating great craftsmen and applying superior methods and materials

Every Friday morning, for over two years now, our entire workforce gathers for a plant update we call “Friday Focus.” Every single associate attends. The objective is for everyone to have the same information and to work together as a team toward our goals. We outline the state of our business, and we talk about issues as we engage in fun, fellowship and

incentives that build the team and help create the ultimate boating experience for our customers. And it works. Improvements in our already excellent quality are exciting. And we make sure our craftsmen have everything they need to contribute all their creative energy, to be passionate about delivering the ultimate boating experience to our customers. Our mission is

for everyone at Grady-White to enjoy coming to work on Monday like they enjoy going home on Friday afternoon. We have a passion for people inside and out—different than any boat company you will find—and because of this, “The Grady Life” is flourishing for our customers.

The Friday Focus is a newer tool that is moving us forward. We are

always looking to improve everything, but part of what we do has stayed the same because some methods and some materials are simply proven successful time and again. Things like:

- ***Hand laid fiberglass provides an unmatched uniform laminate structure. Only 100% hand lay-up gives even thickness on every hull and deck for strength.***



On plant tours (you are invited!) many customers are astonished at how much handwork is done in creating this level of quality. And if you’ve sent us photos (please do), watch out! The lamination or assembly crew may ask you about your family. We share it all.



A little fun and competitive play get everyone focused and engaged for the Friday Focus meeting.



Great coaching and teaching inspire the team’s passion for perfection.





"Build your people and you build your business," are words to live by at Grady-White. Each and every one of these folks—the 50+ members of our Captain's Club—has 25 or more years with our company.

- *Grady-White fabricators have a "kitted" precise pattern for every individual piece of fiberglass, assuring the right amount in exactly the right place. Every part has an optimal glass-to-resin ratio for optimum strength and durability.*
- *Stringer systems: Some of our stringer systems are precision cut by a computerized router, some are special pre-fabricated composites, but all are glassed in while the hull is still in the mold. This means your SeaV<sup>2</sup>® hull will retain the correct, wave-slicing shape. What-ever the*

*material, our tried and proven stringer systems are consistently strong, light, and phenomenally durable.*

- *Sprayed-in closed-cell foam between the stringers adds even more strength.*
- *Transoms are braced with a substantial angled aluminum brace to create an incredibly strong laminated stern structure. The brace supports the motor mounts, transmitting torque throughout the transom.*

Simply put: Nobody builds a tougher boat.

But, again, put all the materials aside.

They are the best, but truth is anybody

A passion for people inside and out—different than any boat company you will find

can buy materials. What really drives the quality, reliability, and safety resulting in the peace of mind in owning a Grady-White are the incredible pride, experience and skill that go into designing and building every boat. Grady-White has consistently worked not merely to better the quality of the boats we build but to build a better,

happier craftsman who creates them. As a result, our proud men and women can do just about anything when it comes to boatbuilding. Come, please, for a factory tour. Then you will see and experience the level of craftsmanship that goes into every Grady-White. You will absolutely be convinced of the Grady-White difference.



Here is the most productive, efficient and completely integrated facility of its kind in the world today, where our company takes the most care and skill in cultivating craftsmen. This is what allows the highest product quality and longest lasting value for you.



# SeaV<sup>2</sup>® hull:

The ultimate big water design

Boat length for boat length, there is no ride like Grady-White's. No other brand offers you anything like the benefits of this clearly superior design. The SeaV<sup>2</sup> is a "continuously variable vee" hull that delivers a soft and stable ride. Unlike other brands, a SeaV<sup>2</sup> hull design has

no two places on the keel where the deadrise is the same. The vee continuously sharpens from the transom to the bow stem. A SeaV<sup>2</sup> hull with 20 degrees of deadrise at the transom will have around 30 degrees amidships—



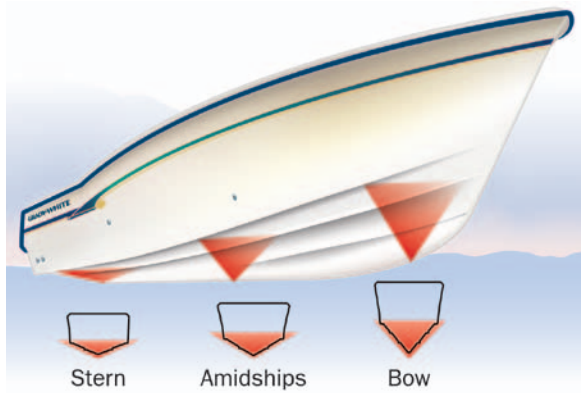
more than even the most radical older deep vee designs. The deeper vee forward means a softer ride at sea. Less vee at the transom coupled with wide chines provide stability at rest and when trolling.

A SeaV<sup>2</sup> hull tracks as if the boat is on rails. The series of wedges that form a SeaV<sup>2</sup> hull has tremendous righting force, giving incredible tracking in quartering seas and especially in following seas. Unlike competitive products, there are no rounded shapes

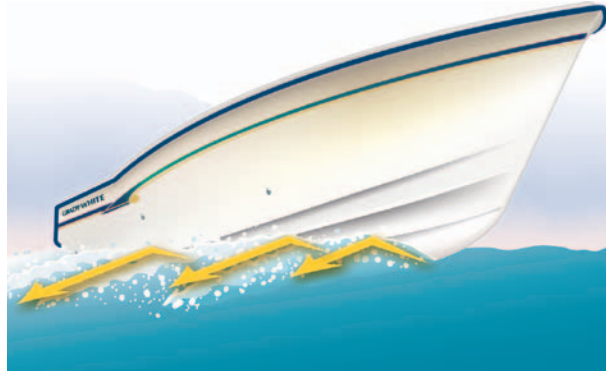


The handling characteristics of Grady-White's SeaV<sup>2</sup> hull design ensure the helmsman of nearly effortless control at any speed and on any course.

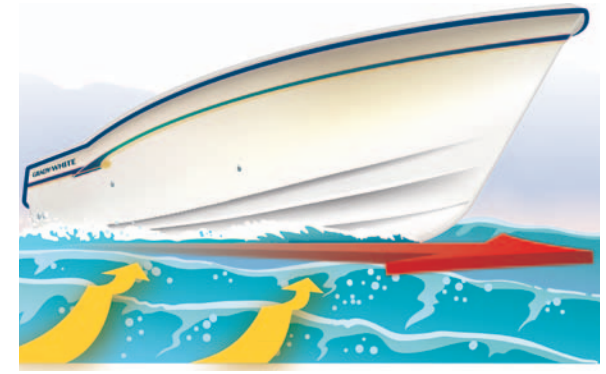




A SeaV²® hull is like a series of sea-cutting wedges. Other hulls do not have a tapering deadrise, bow to stern.



Strakes and chines knock down spray. Notice the three distinct wakes when running in smooth water.



Tracks in quartering seas. The wave cutting ability of this hull makes a Grady-White more efficient and easier to steer.

to behave like a ball in the water, having no upright position.

Strakes and chines knock the spray down cold. Check the spray coming off a SeaV² hull. You can actually see three bow wakes in smooth water. That's the individual deflectors working. This is the reason SeaV² hulls run drier than boats with rounded sections that blow spray past the strakes and chines and into the cockpit.

Grady-White delivers a deep vee ride with modified vee efficiency. Unlike other deep vee hulls, SeaV²

**SeaV² hulls provide a soft ride without fuel efficiency loss.**

hulls provide a soft ride without fuel efficiency loss. A SeaV² hull slices through seas effortlessly with a sharp point of entry, and the varying deadrise builds buoyancy that lifts the boat as it proceeds through waves. Result: better performance. SeaV² hulls are designed exclusively for Grady-White by C. Raymond Hunt Associates, the originators of the modified vee. All of

these features together assure you will have the best day on the water you've ever had, no matter what conditions you encounter.



The key to the SeaV² hull's superior rough water performance is the forward sections that offer a sharp "deep vee" cutting edge and the flatter aft section that provides stability.



# Customer care:

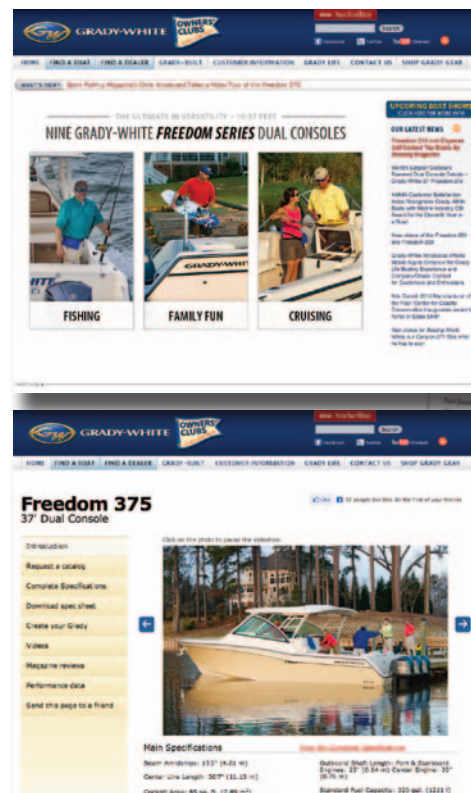
Award-winning people and information at your fingertips

While there is no substitute whatsoever for a visit to your Grady-White dealer or to the Grady-White factory, there's no better spot to find information and inspiration (short of that dealer or plant visit) than [www.gradywhite.com](http://www.gradywhite.com). There you'll learn the most detailed product information that we can possibly provide including up to date specifications, measurements, drawings, layouts, standard features, options, performance reports on all recommended engine offerings, thousands of photos and videos, plus a "Create Your Grady" feature that allows

you to specify your dream boat for dealer contact and pricing. Read published reviews plus check on upcoming boat show dates, locations and show hours. And that's not nearly all. The website helps you find your dealer, and offers mountains of information on the Grady-White company including detailed histories, archived catalogs, owner's manuals and publications, news, schedules and contacts for Club and dealer events, plus customer photo and video galleries, covering every activity.



Our customer relations staff and years of service: (l-r) Danny Davis (26 years), Eddie Rowe (28 years), Bert Kelly (22 years), and Carolyn Ray (25 years)



Visit your dealer or [www.gradywhite.com](http://www.gradywhite.com) for the best information about a Grady-White boat. Subscribe to our publications and download our iPhone app while you are on the website!

Customer help areas include FAQs and searchable Customer Tips, not to mention recommended vendors, plus maintenance products and guides.

While you are there, opt in for our award-winning print newsletter *Anchorline* and award-winning

e-newsletter, *Docklines*, and download our very useful iPhone app. Join thousands who visit every day to learn about Grady-White and the Grady Life. And if

you still need help, you can always contact our unparalleled customer relations team that has over 100 years of combined knowledge about Grady-White boats!

An unsurpassed level of customer care, detailed information, and customer relations experience



# *The Grady Life:* Customers, community and confidence



The Grady Life is all about fun on the water! You can spot a Grady-White a great distance away, and you can be certain there are fun-loving, like-minded people aboard. There really is a rich “Grady family” relationship among fellow owners and enthusiasts, and the company really is like no other brand. Grady-White owners are special. We’ve heard them say over and over, “You

meet the nicest people in a Grady!” That spirit and love of a get-together are part of the reason Grady-White Owners’ Clubs are organized by dealers and owners. Grady Club activities are the hub of activity for The Grady Life and are greatly enjoyed and very successful. Clubs and dealerships sponsor tournaments, seminars, speakers and

more that are not only enjoyable but can inspire confidence to broaden your range and your adventures, whether the next boating event is a local raft-up or multi-day excursion.

Every occasion that brings Grady owners together to share boating experiences gives us the opportunity to learn, laugh and

play alongside our customers. We hope you will ask your dealer about Owners’ Clubs, get-togethers and dealer events in your area, and check the Grady-White website for information and schedules on all events. Fellow Grady owners are also a great source. You may even hear about hot spot fishing locations!

Join the fun! Join The Grady Life!

**Inspires confidence  
to broaden your  
range and  
your adventures**



# Dual Consoles

Customer-designed for big water and the freedom to experience the ultimate in boating versatility



In the early 1970s Grady-White was creating multipurpose fiberglass boats that were tough-built for the wide bays, sounds and rivers from Maine to Miami. These were nearly all carpet- or vinyl-lined day boats under 20 feet in length, but the company began to pack in family features like lockable storage, drink holders, comfortable

seating, unsinkable foam flotation and other items that customers and dealers asked for, creating dual consoles among other designs and cultivating internal know-how for engineering-in versatility for picnicking, waterskiing, fishing, swimming, snorkeling, camping, cruising—the whole

The ultimate in versatility and the perfect activity “transformers”

variety of activities customers enjoy on the water. By the late 1970s, the company began to focus on saltwater fishing boats and was installing easy-to-clean fiberglass liners and saltwater tough features (even heads!) for all-day on big water.

Around 1979, Grady introduced the first true big water dual console. The growing fishing customer base learned the genuine advantages of dual console boats as real competitive “fishing machines,” and provided valuable input. In 1988, Grady-White and C. Raymond Hunt Associates developed Grady’s SeaV<sup>2</sup>

hull, providing the surest handling, most seaworthy and best ride in boating.

Tough-built, multipurpose, big water capable, saltwater fishing savvy, and delivering superb performance and boating’s best ride—the result is today’s Grady-White *Freedom Series* dual consoles, truly the ultimate in versatility and the perfect activity “transformers.” Grady-White builds in the freedom to do anything on the water like no one else possibly can, delivering the best of The Grady Life.





Deluxe cushioned  
surround seating

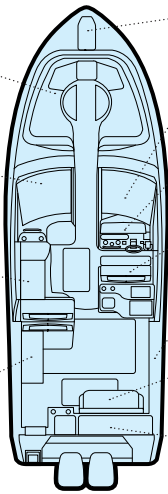
Port console cabin, full  
length sliding berth, aft full  
length berth, TV entertainment  
system, bulk storage

Wraparound companion  
bench seat

Grady-White AV<sup>2</sup><sup>TM</sup> hardtop  
enclosure with sunroof, storage,  
lighting, and optional TV

Electromechanically  
extendable lounge seat

320-gal. fuel capacity



Anchor locker with windlass

Starboard console with head, shower,  
sink, mirror, storage, microwave

Flush mount electronics  
area for two 15" screens

Electromechanically adjustable  
helm seat with dual flip-up bolsters

Optional cockpit side door

Fold away aft bench seat  
(or optional aft wraparound lounge seat)

208-qt. insulated aft box

85-sq. ft. cockpit

## Freedom 375

Beam Amidships: 13' 2" (4.01 m)

Bridge Clearance: 9' 11" (3.02 m)

Center Line Length: 36' 7" (11.15 m)

Hull Draft: 29" (0.74 m)

[gradywhite.com/375](http://gradywhite.com/375)



Maximum HP: 1050 (783 kW)

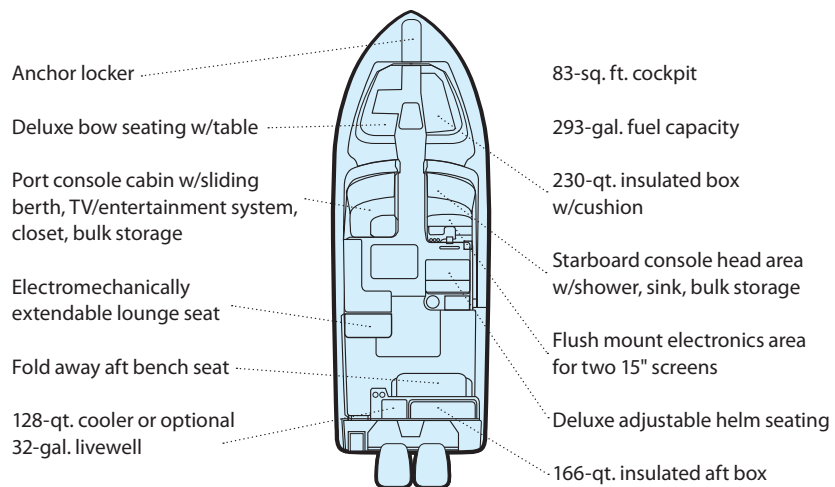
Standard Fuel Capacity: 320 gal. (1211 l)

Weight w/o Engines: 16,250 lb. (7370 kg)



Shown with optional features





## Freedom 335

[gradywhite.com/335](http://gradywhite.com/335)



Beam Amidships: 11' 7" (3.53 m)

Bridge Clearance w/Hardtop: 9' 8" (2.95 m)

Center Line Length: 33' 6" (10.21 m)

Hull Draft: 26" (0.66 m)

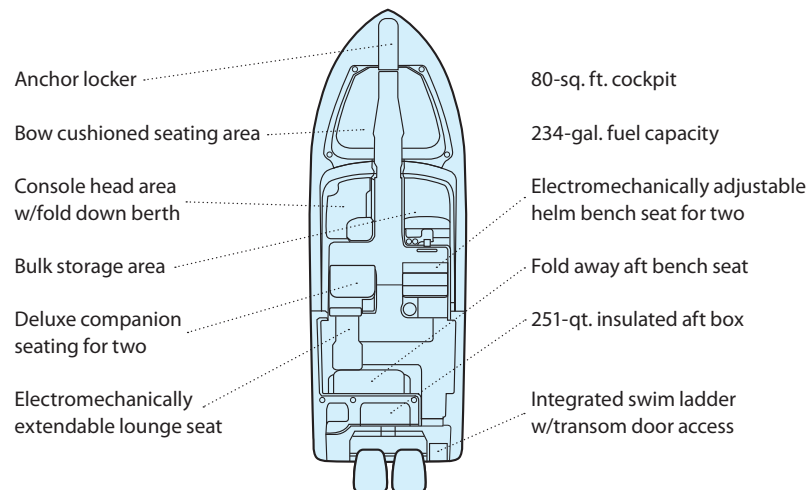
Maximum HP: 700 (522 kW)

Standard Fuel Capacity: 293 gal. (1109 l)

Weight w/o Engines: 11,900 lb. (5398 kg)







## Freedom 307

[gradywhite.com/307](http://gradywhite.com/307)



Beam Amidships: 10' 7" (3.23 m)

Hull Draft: 22" (0.56 m)

Bridge Clearance: 6' 11" (2.11 m)

Maximum HP: 700 (522 kW)

w/Hardtop: 8' 10" (2.69 m)

Standard Fuel Capacity: 234 gal. (886 l)

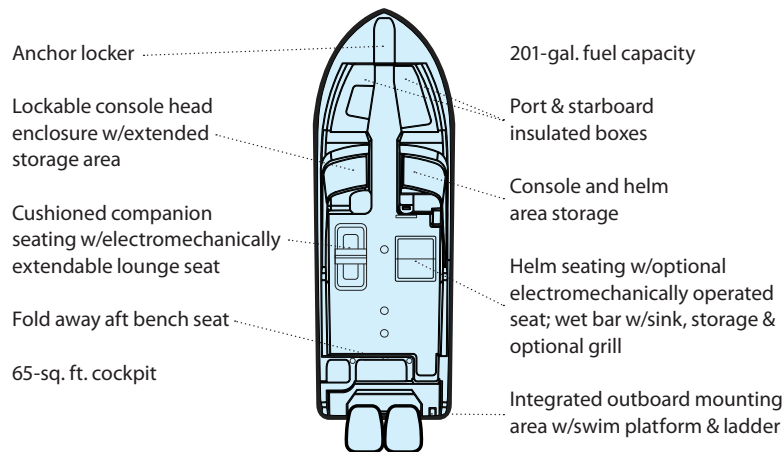
Center Line Length: 30' 6" (9.30 m)

Weight w/o Engines: 7850 lb. (3561 kg)



Shown with optional features





## Freedom 285

[gradywhite.com/285](http://gradywhite.com/285)



Beam Amidships: 9' 6" (2.89 m)

Bridge Clearance: 6' 4" (1.95 m)

w/Hardtop: 8' (2.44 m)

Center Line Length: 28' (8.53 m)

Hull Draft: 20" (.51 m)

Maximum HP: 600 (448 kW)

Standard Fuel Capacity: 201 gal. (761 l)

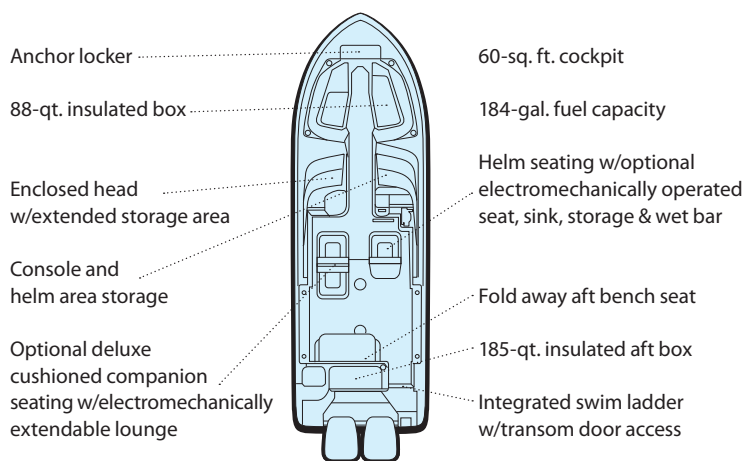
Weight w/o Engines: 5980 lb. (2713 kg)







Shown with optional features



## Freedom 275

Beam Amidships: 8' 6" (2.59 m)

Bridge Clearance: 6' 4" (1.93 m)

w/Hardtop: 8' 6" (2.59 m)

Center Line Length: 26' 11" (8.20 m)

[gradywhite.com/275](http://gradywhite.com/275)



Hull Draft: 20" (0.51 m)

Maximum HP: 500 (373 kW)

Standard Fuel Capacity: 184 gal. (696 l)

Weight w/o Engines: 4972 lb. (2255 kg)



# Freedom 255

gradywhite.com/255



Beam Amidships: 8' 6" (2.59 m)

Bridge Clearance: 5' 7" (1.70 m)

w/Hardtop: 7' 8" (2.34 m)

Center Line Length: 24' 9" (7.55 m)

Hull Draft: 20" (0.51 m)

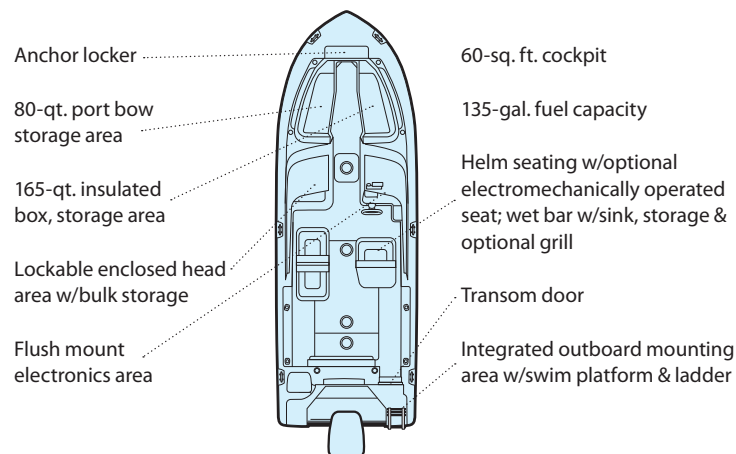
Maximum HP: 400 (298 kW) (rated for F200 engines)

Standard Fuel Capacity: 135 gal. (511 l)

Weight w/o Engine: 4459 lb. (2023 kg)



Shown with optional features



# Freedom 225

gradywhite.com/225



Beam Amidships: 8' (2.44 m)

Bridge Clearance: 5' 6" (1.68 m)

w/Hardtop: 8' 2" (2.44 m)

Center Line Length: 22' 2" (6.76 m)

Hull Draft: 16" (0.41 m)

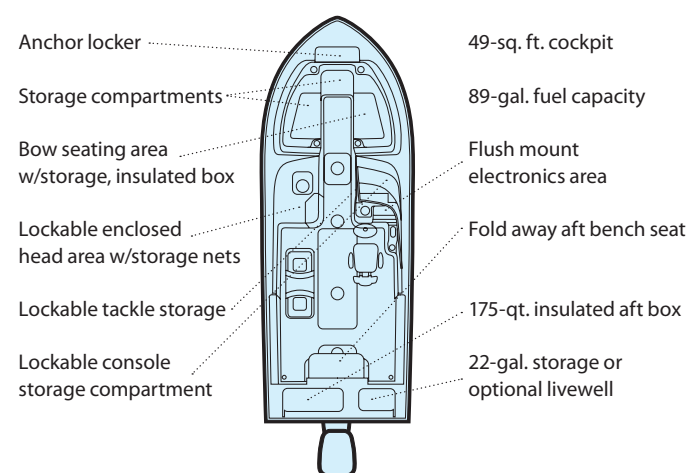
Maximum HP: 300 (224 kW)

Standard Fuel Capacity: 89 gal. (337 l)

Weight w/o Engine: 3450 lb. (1565 kg)



Shown with optional features





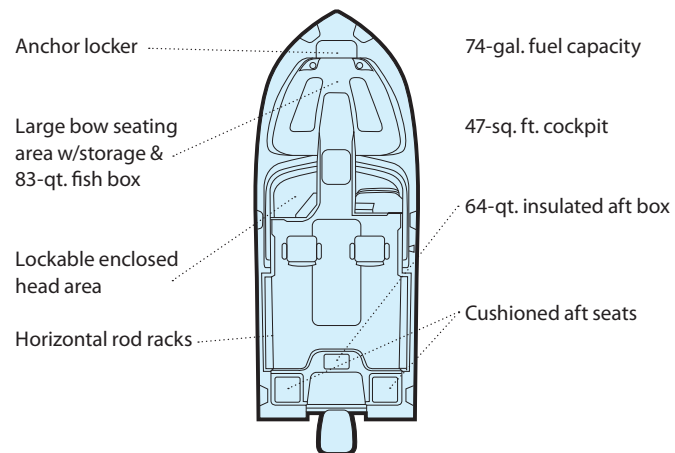
# Freedom 205

gradywhite.com/205



Beam Amidships: 8' 1" (2.46 m)  
 Bridge Clearance: 5' 7" (1.70 m)  
 Center Line Length: 20' 4" (6.20 m)  
 Hull Draft: 15" (0.38 m)

Maximum HP: 250 (187 kW)  
 Standard Fuel Capacity: 74 gal. (280 l)  
 Weight w/o Engine: 2810 lb. (1275 kg)



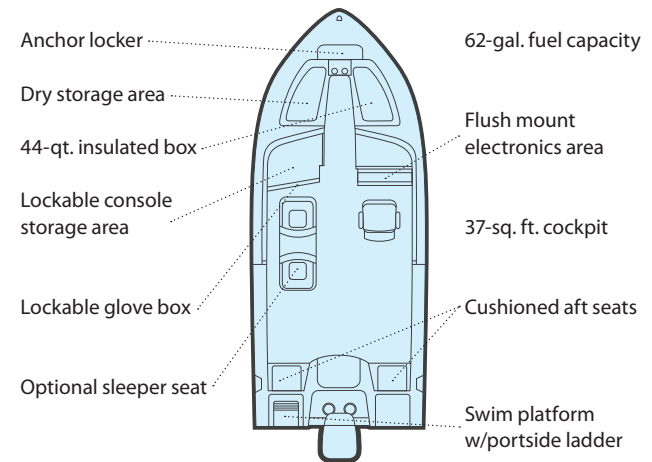
# Freedom 192

gradywhite.com/192



Beam Amidships: 8' (2.44 m)  
 Bridge Clearance: 4' 9" (1.45 m)  
 Center Line Length: 19' 2" (5.84 m)  
 Hull Draft: 14" (0.36 m)

Maximum HP: 200 (149 kW)  
 Standard Fuel Capacity: 62 gal. (235 l)  
 Weight w/o Engine: 2475 lb. (1123 kg)





# Center Consoles

Since 1974, built with the most comfort and highest function thanks to continuous innovation

It surprises many Grady-White enthusiasts, but the truth is that Grady-White has been building center consoles for a very long time. The first Grady-White center console, the *Hatteras 204*, was on the market in 1974 and was created to be a comfortable yet functional design with more customer requested features like rod racks, self-bailing cockpits and insulated fish boxes than offered by other builders. Grady-White saw

an opportunity to create center consoles with more amenities and input-inspired features like ergonomic seating and ample room for navigation and recreational gear.

Customers loved them. Ever since that day 40 years ago, through superb engineering, attention to detail and with the continuous appreciation spanning generations of Grady-White owners, Grady-White

has created the most comfortable, highest fishing-and-family functioning center consoles available. Today's models are the very best built, each

one outclassing anything else in the size length. Every Grady-White center console offers an oversize cockpit with features such as easy access fore and aft, a battleship-tough locking transom door, incomparable tackle and rod storage plus ample lockable bulk storage, and truly comfortable seating including our patented fold away aft bench seat that clears away quickly for fishing. Every single Grady-White center console over 20 feet offers a head area. And every Grady-White center console offers the SeaV<sup>2</sup> ride. It may be a secret to some, but Grady-White is the leading center console builder in the

world today, with more choices and more well refined features than any other manufacturer.

Today's Grady-White center consoles offer go-anywhere, do-anything performance, with the emphasis on fishing.

More choices  
and more  
well-refined  
features than  
any other



1974 Hatteras 204

Even in 1974: comfortable seating, rod racks and holders, casting platform and insulated cooler capacity





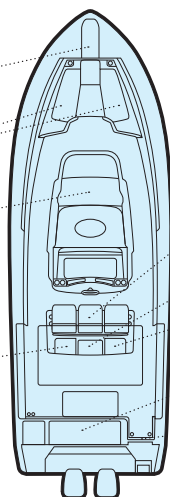


Anchor locker w/push  
button power windlass

Two 263-qt. fish boxes

Lockable center cabin area  
w/head, stand-up shower, double  
berth, bulk storage, rod storage &  
entertainment center (60 sq. ft.)

164-qt. insulated box  
or optional additional  
41-gal. livewell



85-sq. ft. cockpit

390-gal. fuel capacity

Deluxe lean bar w/three  
helm chairs, footrests and  
flip-up bolsters

Rigging station w/sink or  
optional grill

291-qt. aft fish box

Transom door

## Canyon 376

[gradywhite.com/376](http://gradywhite.com/376)

Beam Amidships: 13' 2" (4.01 m)

Bridge Clearance w/T-top: 9' (2.74 m)

Center Line Length: 36' 7" (11.15 m)

Hull Draft: 29" (0.74 m)

Maximum HP: 1050 (783 kW)

Standard Fuel Capacity: 390 gal. (1476 l)

Weight w/o Engines: 12,850 lb. (5829 kg)







Shown with optional features

## Canyon 336

[gradywhite.com/336](http://gradywhite.com/336)



Beam Amidships: 11' 7" (3.53 m)

Bridge Clearance: 8' (2.44 m)

w/T-top: 9' 2" (2.79 m)

Center Line Length: 33' 6" (10.21 m)

Hull Draft: 25" (0.64 m)

Maximum HP: 700 (522 kW)

Standard Fuel Capacity: 333 gal. (1260 l)

Weight w/o Engines: 9200 lb. (4173 kg)

Anchor locker

165-qt. port and  
starboard fish boxes

Console seating

Lockable console w/stand up  
head area, shower, sink, berth,  
rod storage and storage drawers

Helm with electromechanically  
operated electronics enclosure

80-sq. ft. cockpit

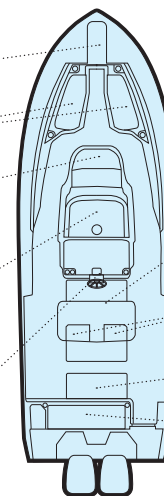
333-gal. fuel capacity

Deluxe lean bar w/three  
helm chairs, footrests and  
flip-up bolsters

Rigging station w/freshwater  
washdown and 45-gal. livewell

Fold away aft bench seat

291-qt. aft fish box





Anchor locker

150-qt. port and  
starboard fish boxes

Lockable console with  
stand up head area, shower,  
sink and storage areas

Electromechanically oper-  
ated electronics enclosure

75-sq. ft. cockpit

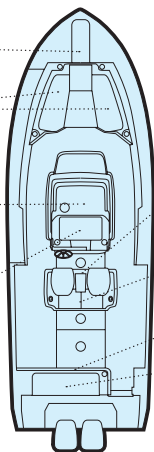
290-gallon fuel capacity

Deluxe lean bar w/two  
helm chairs, footrests and  
flip-up bolsters

Rigging station w/freshwater  
sink and 47-gal. livewell

Fold away aft bench seat

304-qt. aft fish box



## Canyon 306

[gradywhite.com/306](http://gradywhite.com/306)



Beam Amidships: 10' 7" (3.23 m)

Bridge Clearance: 7' 3" (2.21 m)

w/T-top: 9' 4" (2.84 m)

Center Line Length: 30' 6" (9.30 m)

Hull Draft: 21" (0.53 m)

Maximum HP: 700 (522 kW)

Standard Fuel Capacity: 290 gal. (1098 l)

Weight w/o Engines: 6950 lb. (3152 kg)



Shown with optional features



# Canyon 283

[gradywhite.com/283](http://gradywhite.com/283)



Beam Amidships: 9' 6" (2.90 m)

Bridge Clearance: 7' 7" (2.31 m)

w/T-top: 9' (2.74 m)

Center Line Length: 28' (8.53 m)

Hull Draft: 20" (0.51 m)

Maximum HP: 600 (448 kW)

Standard fuel Capacity: 191 gal. (723 l)

Weight w/o Engines: 5864 lb. (2660 kg)

Anchor locker

Two 135-qt. fish boxes

Electromechanically operated electronics enclosure

Deluxe lean bar w/footrest, backrest and flip-up bolster

Lockable rod storage

69-sq. ft. cockpit

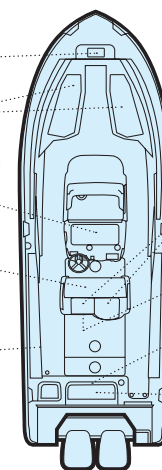
191-gal. fuel capacity

Rigging station w/freshwater sink and 45-gal. livewell

Tackle locker w/trays

Fold away aft bench seat

265-qt. aft fish box w/removable partition and ob drain



Shown with optional features







## Canyon 271

[gradywhite.com/271](http://gradywhite.com/271)



Beam Amidships: 9' 6" (2.89 m)

Bridge Clearance: 6' 11" (2.11 m)

w/T-top: 8' 2" (2.49 m)

Center Line Length: 26' 10" (8.18 m)

Hull Draft: 23" (0.58 m)

Maximum HP: 600 (448 kW)

Standard Fuel Capacity: 186 gal. (704 l)

Weight w/o Engines: 5790 lb. (2626 kg)

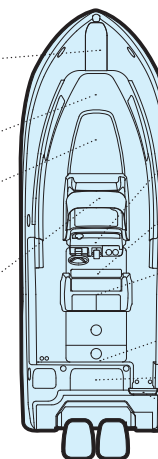
Anchor locker  
w/rode storage

Wide walkway around  
bow at deck level

Bow cushioned  
lounging seating w/389-qt.  
insulated fish box

Lockable console  
w/stand-up head area, bulk  
storage and marine head

65-sq. ft. cockpit



186-gal. fuel capacity

Flush mount electronics area

Deluxe lean bar w/footrests,  
backrest and flip-up bolsters

Rigging station w/freshwater sink  
and 47-gal. livewell

Fold away aft bench seat

198-qt. insulated aft fish box

Transom door



# Fisherman 257

gradywhite.com/257



Beam Amidships: 8' 6" (2.59 m)

Bridge Clearance: 7' (2.13 m)

w/Hardtop: 8' 6" (2.59 m)

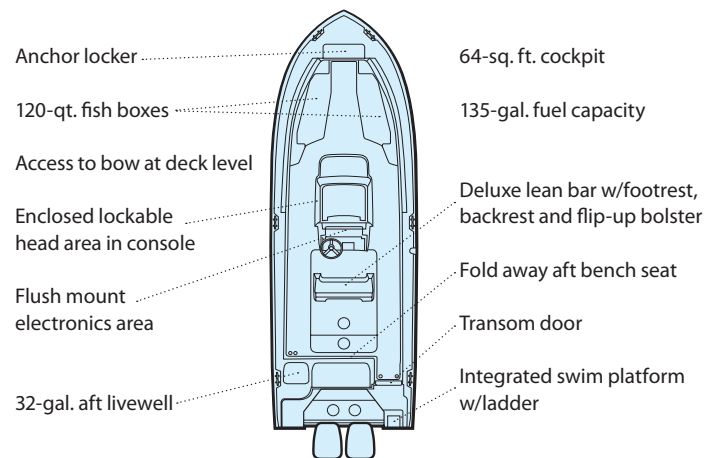
Center Line Length: 24' 9" (7.55 m)

Hull Draft: 20" (0.51 m)

Maximum HP: 400 (298 kW) (rated for F200 engines)

Standard Fuel Capacity: 135 gal. (511 l)

Weight w/o Engine: 4300 lb. (1951 kg)



# Fisherman 230

gradywhite.com/230



Beam Amidships: 8' 6" (2.59 m)

Bridge Clearance: 6' 8" (1.88 m)

w/T-top: 8' 5" (2.57 m)

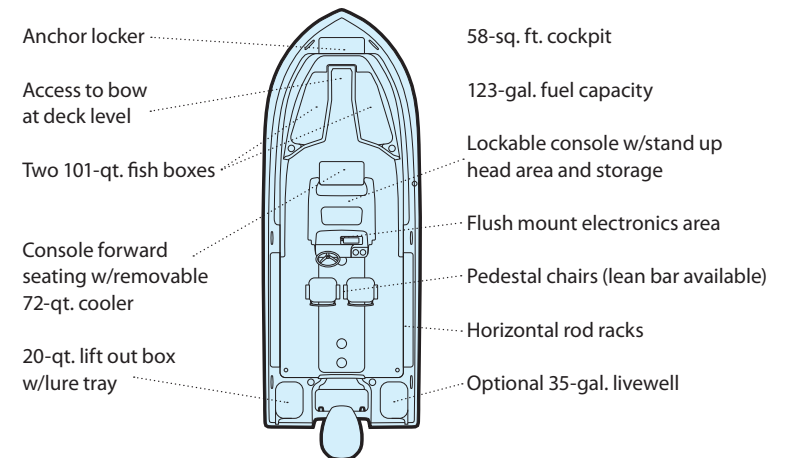
Center Line Length: 22' 8" (6.9 m)

Hull Draft: 20" (0.51 m)

Maximum HP: 300 (224 kW)

Standard Fuel Capacity: 123 gal. (465 l)

Weight w/o Engine: 4002 lb. (1815 kg)





# Fisherman 209

gradywhite.com/209



Beam Amidships: 8' 1" (2.46 m)

Bridge Clearance: 6' 4" (1.95 m)

w/T-top: 7' 9" (2.36 m)

Center Line Length: 20' 4" (6.20 m)

Hull Draft: 15" (0.38 m)

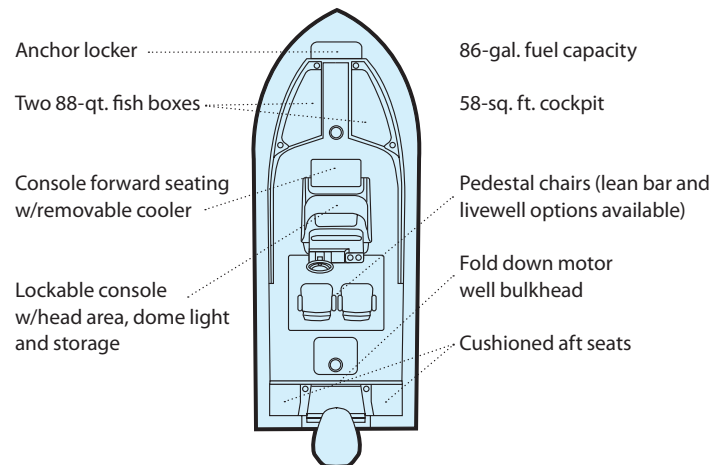
Maximum HP: 250 (187 kW)

Standard Fuel Capacity: 86 gal. (325 l)

Weight w/o Engine: 2600 lb. (1179 kg)



Shown with optional features



# Fisherman 180

gradywhite.com/180



Beam Amidships: 7' 5" (2.26 m)

Bridge Clearance: 5' 7" (1.70 m)

Center Line Length: 17' 10" (5.44 m)

Hull Draft: 15" (0.38 m)

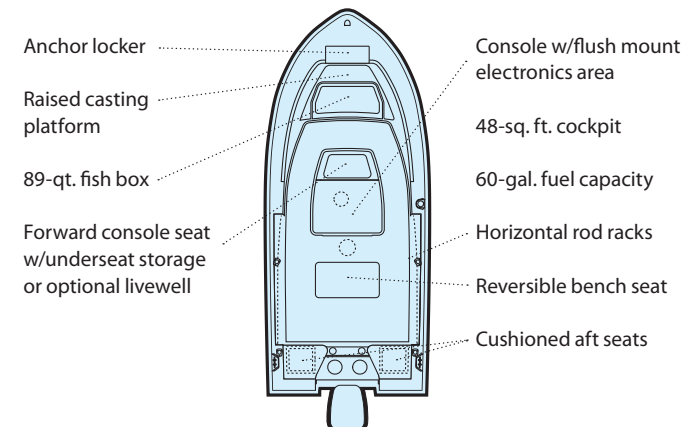
Maximum HP: 150 (112 kW)

Standard Fuel Capacity: 54 gal. (204 l)

Weight w/o Engine: 2150 lb. (975 kg)



Shown with optional features





# Walkaround Cabins

We pioneered the design, then perfected it with customer dream lists over the past 40 years.



After seeing a multihull version of a “walkaround cabin” in January 1974, the Grady-White team decided to come up with a single hull version in this style, knowing that such a design offered benefits that lots of customers wanted. So we created a 20-foot

cuddy cabin you could literally walk around. It quickly

became the company’s biggest seller. This design provided space for a real windshield with full weather protection. The introduction of the walkaround cabin gave Grady owners the benefit of a console plus a cabin with room for a bunk and even a head, while still allowing easy movement fore and aft. Today’s Grady-White walkarounds are the product of 40 years of ongoing customer recommendations. Every walkaround model today is a proven terrific sportfishing superstar, complete with what the fisherman wants and needs including insulated

Forty years of ongoing customer recommendations

fish and iceboxes, lockable storage and cleverly built-in

room for rods and tackle. Helm stations accommodate the very latest in marine electronics, navigation and systems controls. Again through boat owners’ requests, Grady-White walkarounds feature truly comfortable and innovative seating. Every Grady-White walkaround has a head or head option. Handsome cabin

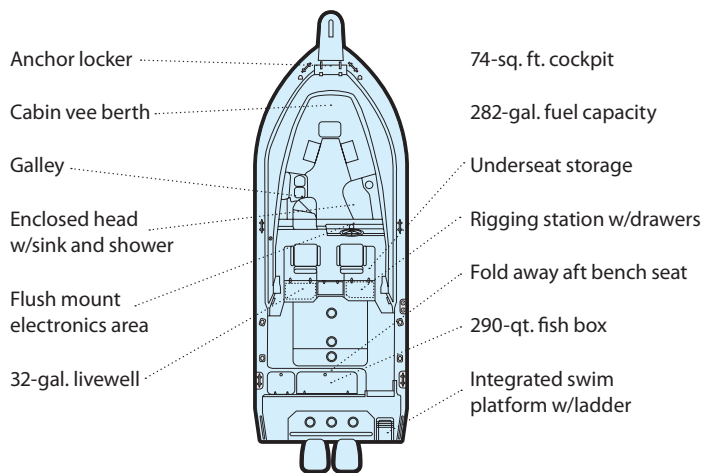
accommodations have been tweaked to perfection within the bounds of model size to offer comfortable lounging and sleeping areas, exceptional storage, plus galleys and more on larger models. Thanks to decades of customer inspiration, Grady-White walkarounds are one of the company’s signature designs even today.



Today’s *Chesapeake 290* walkaround cabin. Ergonomically arranged and feature rich, this is cabin engineering at its best.



Shown with optional features



## Marlin 300

[gradywhite.com/300](http://gradywhite.com/300)



Beam Amidships: 10' 7" (3.23 m)

Hull Draft: 23" (0.58 m)

Bridge Clearance w/Hardtop: 9' 9" (2.97 m)

Maximum HP: 700 (522 kW)

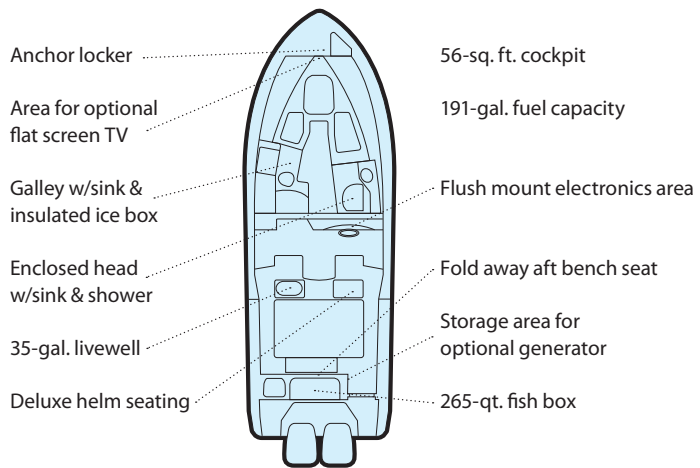
Center Line Length: 30' 6" (9.30 m)

Standard Fuel Capacity: 282 gal. (1067 l)

Length Overall (w/pulpit): 32' 7" (9.93 m)

Weight w/o Engines: 8221 lb. (3729 kg)





## Chesapeake 290

[gradywhite.com/290](http://gradywhite.com/290)



Beam Amidships: 9' 11" (3.02 m)

Hull Draft: 24" (0.6 m)

Bridge Clearance w/Hardtop: 9' 9" (2.97 m)

Maximum HP: 600 hp (448 kW)

Center Line Length: 28' 6" (8.69 m)

Standard Fuel Capacity: 191 gal. (723 l)

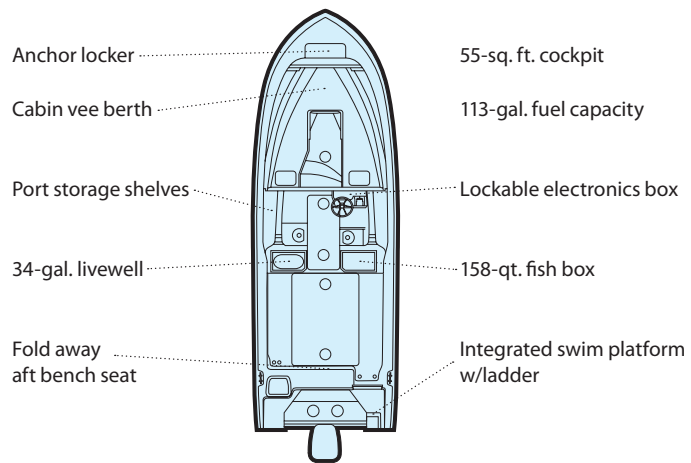
Length Overall (w/pulpit): 29' 9" (9.07 m)

Weight w/o Engines: 7650 lb. (3470 kg)

Shown with optional features







## Journey 258

[gradywhite.com/258](http://gradywhite.com/258)



Beam Amidships: 8' 6" (2.59 m)

Bridge Clearance: 6' 6" (1.98 m)

w/Hardtop: 8' 8" (2.64 m)

Center Line Length: 24' 9" (7.55 m)

Hull Draft: 20" (0.51 m)

Maximum HP: 400 (298 kW) (rated for F200 engines)

Standard Fuel Capacity: 113 gal. (428 l)

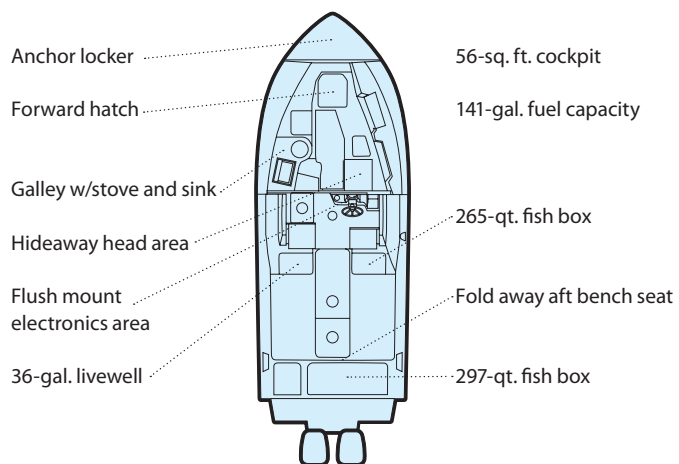
Weight w/o Engine: 4320 lb. (1960 kg)







Shown with optional features



## Gulfstream 232

[gradywhite.com/232](http://gradywhite.com/232)



Beam Amidships: 9' 3" (2.82 m)

Bridge Clearance: 6' 5" (1.96 m)

w/Hardtop: 8' 8" (2.64 m)

Center Line Length: 23' 5" (7.14 m)

Hull Draft: 20" (0.51 m)

Maximum HP: 400 (298 kW)

Standard Fuel Capacity: 141 gal. (534 l)

Weight w/o Engine:

232 (single): 4538 lb. (2058 kg)

232 (dual): 4605 lb. (2089 kg)

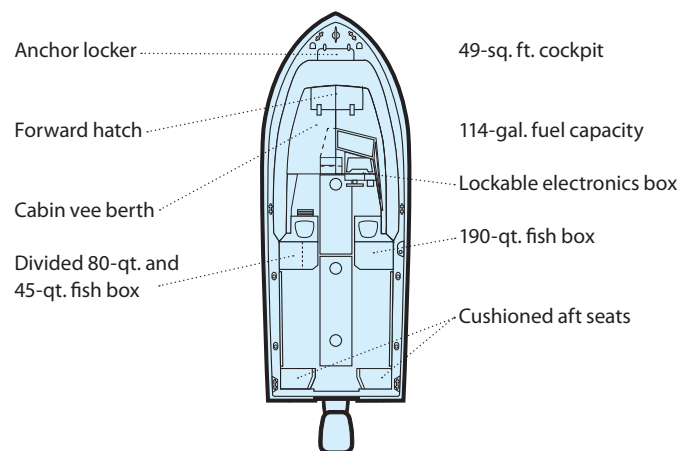


## Seafarer 226/228

gradywhite.com/226



Beam Amidships: 8' (2.44 m)	Maximum HP: 300 (224 kW)
Bridge Clearance: 6' 2" (1.88 m)	Standard Fuel Capacity: 114 gal. (431 l)
w/Hardtop: 8' 4" (2.54 m)	Weight w/o Engine:
Center Line Length: 22' 2" (6.76 m)	226 (standard transom): 3385 lb. (1535 kg)
Hull Draft: 16" (0.41 m)	228 (Grady Drive transom): 3510 lb. (1592 kg)

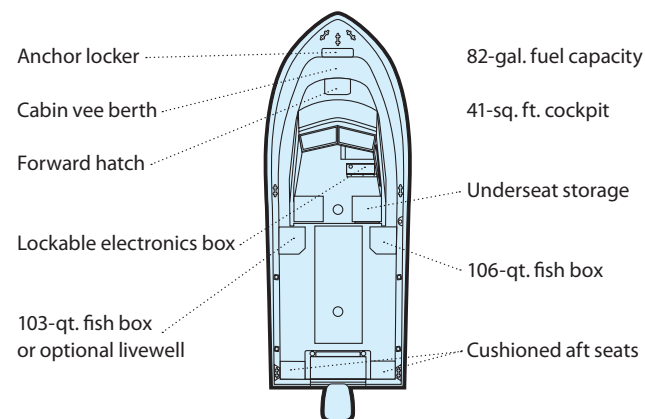


## Adventure 208

gradywhite.com/208



Beam Amidships: 8' 1" (2.46 m)	Hull Draft: 15" (0.38 m)
Bridge Clearance: 5' 9" (1.75 m)	Maximum HP: 250 (187 kW)
w/Hardtop: 8' (2.44 m)	Standard Fuel Capacity: 82 gal. (310 l)
Center Line Length: 20' 4" (6.20 m)	Weight w/o Engine: 2900 lb. (1315 kg)





# Express Cabins

Fishing and cruising capability with interior spaciousness and outboard power advantage

Just as Grady-White has continuously improved its entire product line, the company's now-exclusive power partner, Yamaha Outboards, always makes strides. In 2001 Yamaha introduced the new Saltwater Series II with increased performance and high horsepower ratings. Grady-White saw the opportunity for a market breakthrough and recognized the advantage of these outboards, and so introduced the *Express 330*.

That year the 330 was named *Boating* magazine's Boat of the Year and since then, one of the Top Boats of the Decade.

Breakthrough design, more comfortable and inviting than ever!

"Every once in a while a boat model is introduced that stands out in so many ways it becomes a 'category killer.' These are boats that changed the game in the niche they occupy—often rippling other niches as well." And that was just the beginning of the accolades for Grady's *Express* design. Thanks to the popularity of the design, Grady-White *Express* models continue to change the game. All models have optimal

range, fuel economy, comfort and fishing/cruising abilities beyond any serious inboard sportfish of comparable length.

Yet today's Grady-White *Express* models offer great speed and performance, interior volume, running angle and ease of use with today's better-than-ever outboards and docking systems. Families agree:

features like the helm area "surround seating" are more inviting than ever. Grady-White *Express* models including the latest *Express 370* continue to be breakthrough boats offering tremendous value and family comfort.

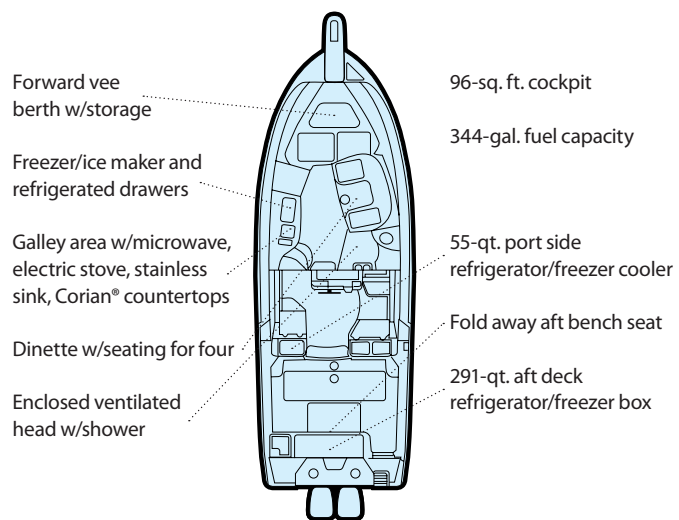


A place for everything and everything in its place; the *Express 330* cabin interior.





Shown with optional features



## Express 370

[gradywhite.com/370](http://gradywhite.com/370)



Beam Amidships: 13' 2" (4.01 m)

Hull Draft: 29" (0.74 m)

Bridge Clearance w/Hardtop: 10' 6" (3.20 m)

Maximum HP: 1050 (783 kW)

Center Line Length: 36' 7" (11.15 m)

Standard Fuel Capacity: 344 gal. (1302 l)

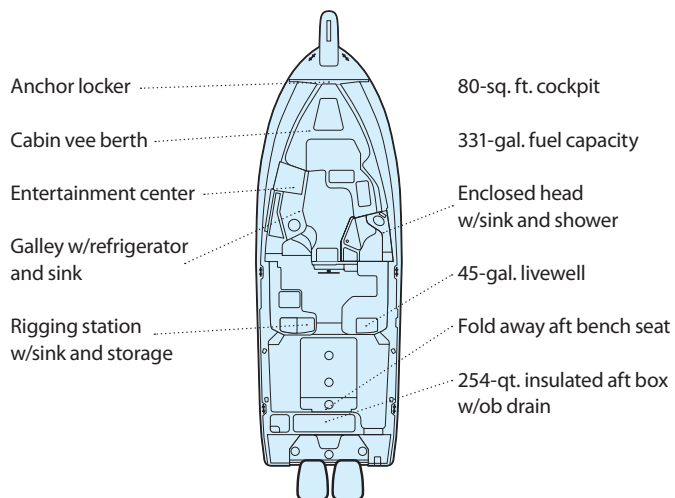
Length Overall (w/pulpit): 39' 3" (11.96 m)

Weight w/o Engines: 14,919 lb. (6767 kg)





Shown with optional features



## Express 330

[gradywhite.com/330](http://gradywhite.com/330)



Beam Amidships: 11' 7" (3.53 m)

Hull Draft: 25" (0.64 m)

Bridge Clearance w/Hardtop: 9' 10" (3.00 m)

Maximum HP: 700 (522 kW)

Center Line Length: 33' 6" (10.21 m)

Standard Fuel Capacity: 331 gal. (1253 l)

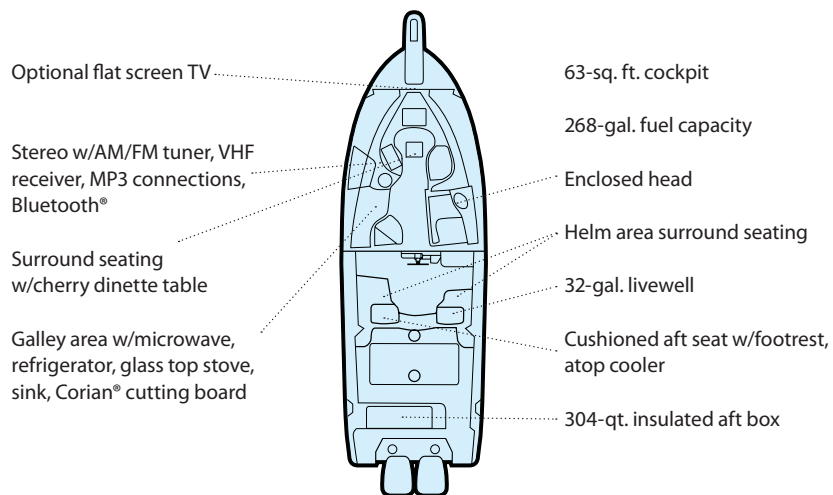
Length Overall (w/pulpit): 35' 10" (10.92 m)

Weight w/o Engines: 10,840 lb. (4917 kg)





Shown with optional features



## Express 305

[gradywhite.com/305](http://gradywhite.com/305)



Beam Amidships: 10' 7" (3.23 m)

Hull Draft: 23" (0.58 m)

Bridge Clearance w/Hardtop: 9' 9" (2.97 m)

Maximum HP: 700 (522 kW)

Center Line Length: 30' 6" (9.30 m)

Standard Fuel Capacity: 268 gal. (1014 l)

Length Overall (w/pulpit): 32' 7" (9.93 m)

Weight w/o Engines: 8850 lb. (4014 kg)



# Sample Options



See [gradywhite.com](http://gradywhite.com) for all features and options including those highlighted here.



Fiberglass T-top with spreader lights (271, 283, 306, 336; standard on 376)



Hardtop with radio box and spreader lights (208, 226, 228, 232, 258; standard on 290, 300, 330, 370)



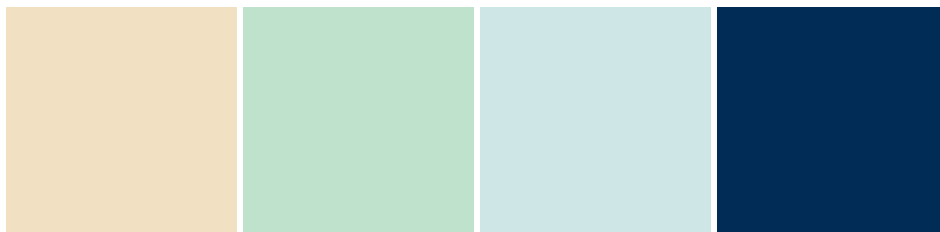
Hardtop with enclosed helm (330, 370)



Freedom Series hardtop with storage net, spreader lights, outrigger plates (225, 255, 275, 285, 307; standard on 335). AV<sup>2</sup>™ hardtop standard on 375 (see pg 13).



With more requests for colored hulls, and taking into account the individual personalities of today's customers, Grady-White now offers three colors of hull paint and a very sharp-looking gelcoat color as factory options on every boat, in addition to our classic Grady-White cream-colored hull.



▲ Sand (gelcoat)

▲ Sea Glass (paint)

▲ Vista Blue (paint)

▲ Harbor Blue (paint)

\*Ink colors shown are approximate. See your dealer for actual color samples.



Freedom Series sleeper seat with storage (192, 205, 225, 255)



Platinum helm chair



Deluxe I helm or companion chair



Deluxe II helm or companion chair



Deluxe III helm or companion chair with flip-up bolster



366 motorized bow table. Manual bow and/or cockpit tables are available options (205, 225, 255, 257, 283, 306, 376; standard on 275, 285, 307, 335). Tables vary per model.



Grill for wet bar or lean bar, replaces sink (255, 275, 285, 307, 335, 370, 376). Standard on 375, in addition to sink.

**Many more options are available than shown here.**  
See [gradywhite.com](http://gradywhite.com) for complete listings of standard features and options.



Bow casting platform insert with cushions (209, 230, 257, 283, 306, 336, 376); sun platform insert with cushions (205, 225, 255, 275, 285, 307, 335)



Yamaha® Helm Master™ fully integrated boat control system (283, 285, 290, 300, 305, 306, 307, 330, 335, 336, 370, 375, 376)



# The ultimate in boating satisfaction. The ultimate in versatility, ease of use, comfort, safety and convenience.

- 1. The bow anchor locker** allows quick access to the anchor for safety. Many models have power windlasses with remote switches.
- 2. Flush mount electronics areas** are standard on most models.
- 3. Comfortable seating** throughout the boat is second to none. Our deluxe seating is fully adjustable for additional comfort. Flip-up seat bolsters are available on many models.
- 4. Sound systems** are available on nearly all Gradys. They are standard on our larger models.
- 5. Dry storage.** Every Grady has plenty, just what you need to enjoy many activities on the water. Room for tow tools and toys, fishing and dive gear, picnic supplies, towels, extra clothing, personal items and more.
- 6. Easy to maintain and clean, stainless steel lined drink holders** are everywhere you need them. Convenient, and fewer slippery spills for safety!
- 7. Storage nets** are easy to use, easy to get to and perfect for stowing lots of items you can see and quickly locate, especially personal flotation devices (PFDs) in the bigger nets!
- 8. Enclosed head area.** Every Grady-White 20 feet and over offers a head or head option. That's great comfort for all-day adventures.
- 9. Toe rails** are easy to maintain and provide additional safety when you lean over the gunwale.
- 10. Take the rods along every trip!** There's room for plenty of rods on every Grady-White. Cabin models, T-tops and hardtops often have additional rod racks.
- 11. Patented fold away aft seating** is available on most models, opening up more cockpit room. Big cockpits are a Grady-White feature.



Freedom 285  
Shown with optional features

- 12. Grady-White's nonskid surfaces** are everywhere you step and really provide traction.
- 13. Tackle storage** and rigging areas are located on most models.
- 14. Insulated ice boxes and fish boxes** drain directly overboard by gravity on every Grady-White.
- 15. Handholds** are where you naturally reach.
- 16. Safety windshield** (some models have unbreakable acrylic)
- 17. True self-bailing** boat directs water overboard by gravity whether at rest or underway. All scuppers drain overboard. Inspection ports are watertight. All Grady-Whites have automatic bilge pumps, with two pumps on most models over 20 feet. Our dedication to self-bailing cockpit integrity is unparalleled.
- 18. All Grady-Whites have foam sprayed into the hull for level or basic flotation**, even though not required in boats 20 feet and over. This foam also adds strength, thermal insulation and deadens sounds. There is no boat brand with better flotation properties.
- 19. Through-bolted 316 stainless steel hardware** is secured with locking nuts.
- 20. Heavy-duty fiberglass transom door** is not only for big fish, but is a safety feature and provides easy access for tow sports, swimming, snorkeling, and more. Solid, 316 stainless transom door hardware is highlighted by a destroyer-sized latch with a positive locking mechanism for added safety.
- 21. Aluminum transom brace** is bonded to the top of outboard transoms, which reinforces transom strength and distributes motor torque. On integrated bracket models, the brace is through bolted to the longitudinal stringer system.

**You're in command. Boat Smart from the Start!** Take a boating education course, and get a vessel safety check for your boat. For more information, contact: U.S. Coast Guard Auxiliary 877-875-6296/800-368-5647 or [www.cgaux.org](http://www.cgaux.org) and U.S. Power Squadrons 888-367-8777 or [www.usps.org](http://www.usps.org).

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**5-YEAR**  
STRUCTURAL  
HULL WARRANTY  
See your dealer for details  
about our warranties.



For smartphones

**Mixed Sources**  
Product group from well-managed forests, controlled sources and recycled wood or fiber  
[www.fsc.org](http://www.fsc.org) Cert no. SW-COC-002351  
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The NMMA Certified label on a Grady-White boat is your proof it meets, and often exceeds, industry quality and safety standards.

